

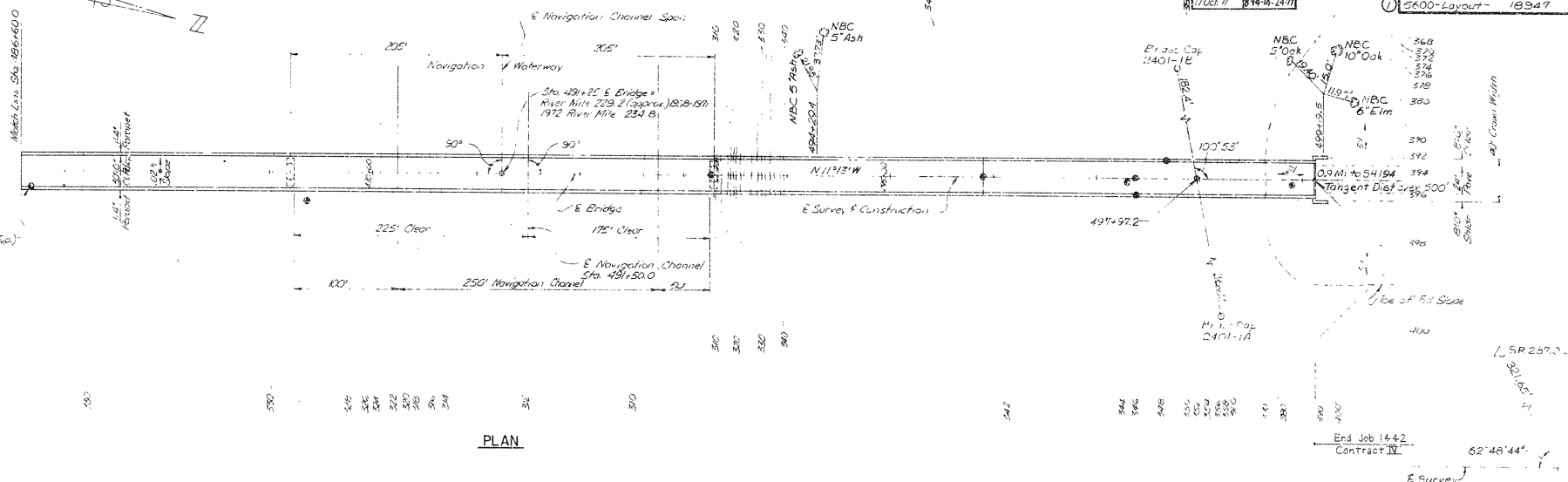
SHEET 6 OF 7

LAYOUT OF BRIDGE OVER ARKANSAS RIVER
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
LOGAN & JOHNSON CO.
ROUTE 109 SEC. 38 & 4
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.

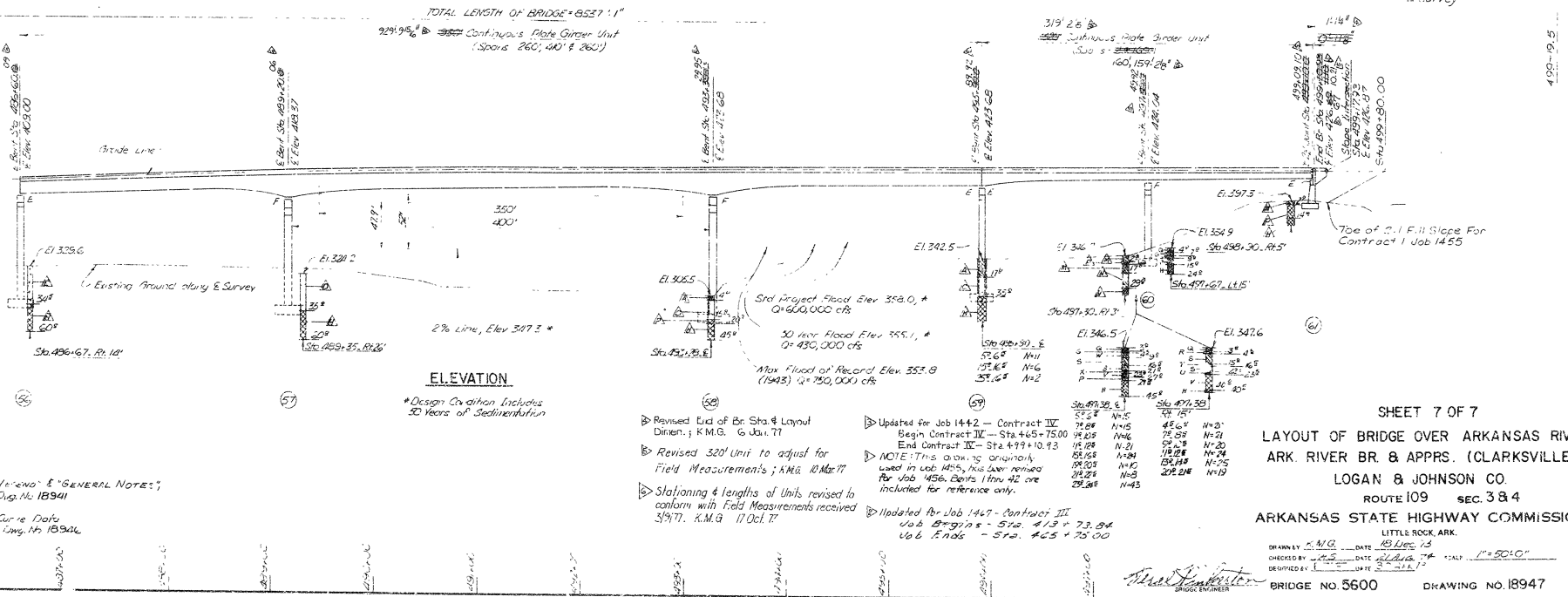
DESIGNED BY: KMG DATE: 10/26/73 SCALE: 1"=50'-0"
CHECKED BY: HGS DATE: 11/1/73
REVIEWED BY: LTF DATE: 3/24/75
BRIDGE NO. 5600 DRAWING NO. 18946

Match Line Skz. 286+600

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	PER ROAD SHEET NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
5 MAY 73		10-4-76	920-104-76	5	ARK.	TQS-A12811		
11-3-75		1-5-77	30-1-77					
8-17-76	7-8-77	11-77	504-11-77	JOB NO.		1442		
17 Oct 77	8-14-16-24-77			①	5600-Layout-	18947		



PLAN



ELEVATION

SHEET 7 OF 7

LAYOUT OF BRIDGE OVER ARKANSAS RIVER
ARK. RIVER BR. & APPRS. (CLARKSVILLE)

LOGAN & JOHNSON CO.

ROUTE 109 SEC. 3 & 4

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY M. M. DATE 13 Dec 73
CHECKED BY M. S. DATE 21 Dec 73 SCALE 1"=50' 0"
DESIGNED BY E. J. DATE 3 Jan 74

BRIDGE NO. **5600** DRAWING NO. **18947**

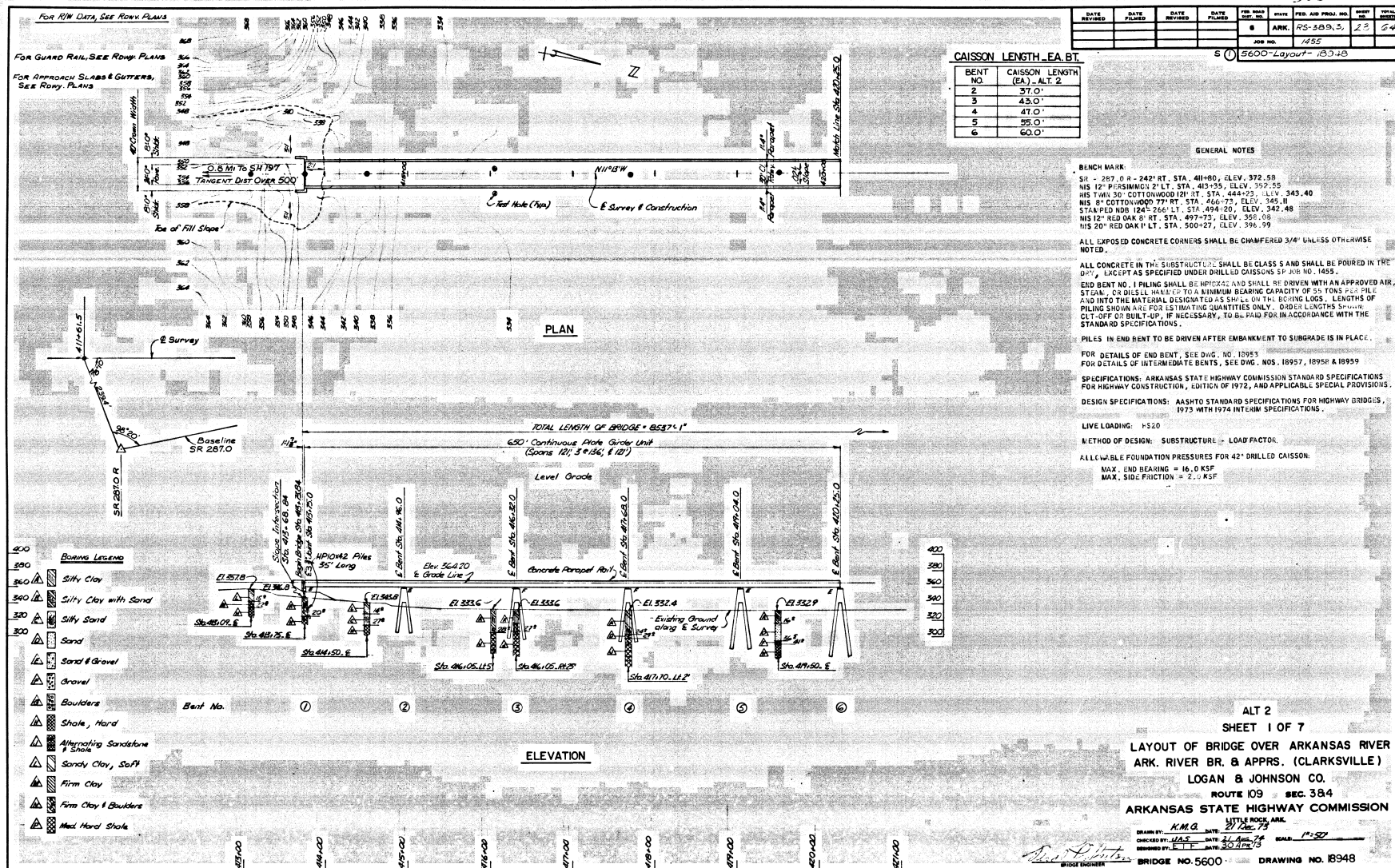
NOTE. For "BORING LOGS" & "GENERAL NOTES";
see Sheet 1, Div. No 18941

NOTE: For Vertical Curve Data
see Sheet 6, Dwg. No 18946

- ▷ Revised End of Br. Sta. & Layout
Dinmen.; K.M.G. 6 Jan. 77
- ▷ Revised 320' Unit to adjust for
Field Measurements; K.M.G. 10 Mar. 77
- ▷ Stationing & Lengths of Units revised to
conform with Field Measurements received
3/9/77. K.M.G. 17 Oct. 77

▶ Updated for Job 1442 - Contract IV
 Begin Contract IV - Sta. 465+75.00
 End Contract IV - Sta. 499+0.93
 ▶ NOTE: This work is originally
 used in Job 1456, has been revised
 for Job 1456. Cmts 1 thru 42 are
 included for reference only.

▶ Updated for Job 1467 - Contract III
 Job Begins - Sta. 413+0
 Vol Ends - Sta. 465+



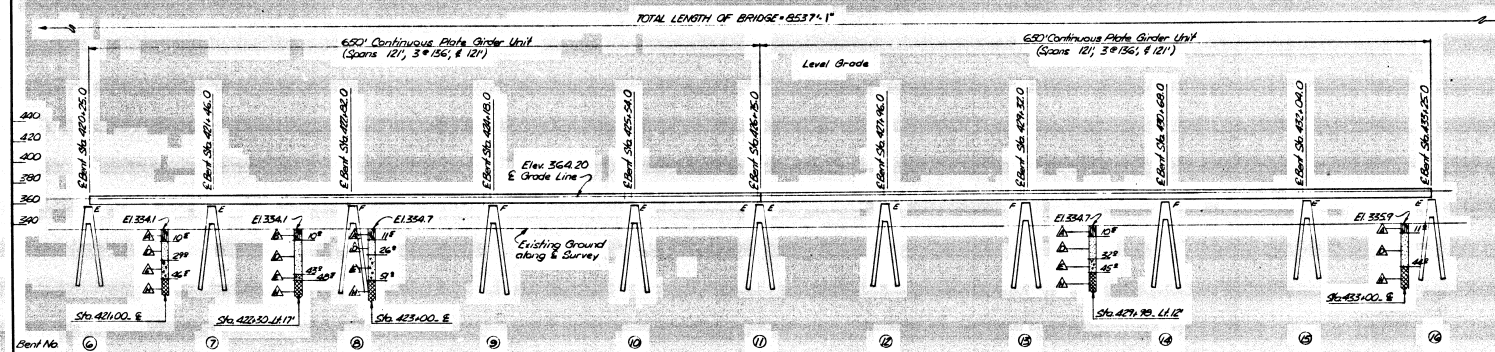
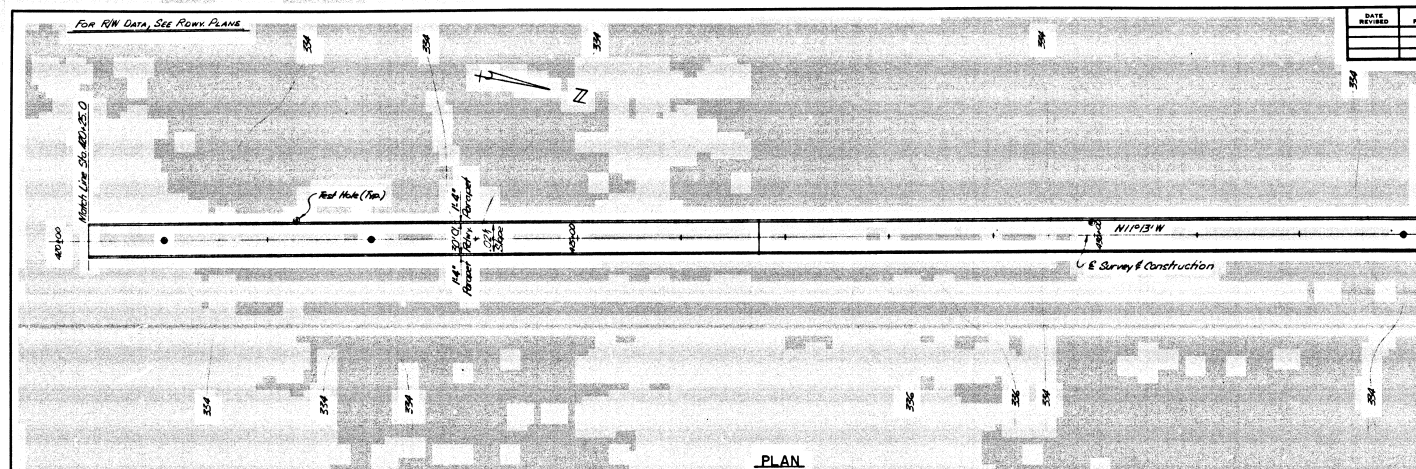
299

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	REV. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.	RS-583.2	24	24
						1455		

5600-Layout-18949

CAISSON LENGTH - EA. FT.

BENT NO.	CAISSON LENGTH (EA.) ALT. 2
6	60.0'
7	63.0'
8	66.0'
9	65.0'
10	64.0'
11	62.0'
12	61.0'
13	63.0'
14	63.0'
15	59.0'
16	60.0'



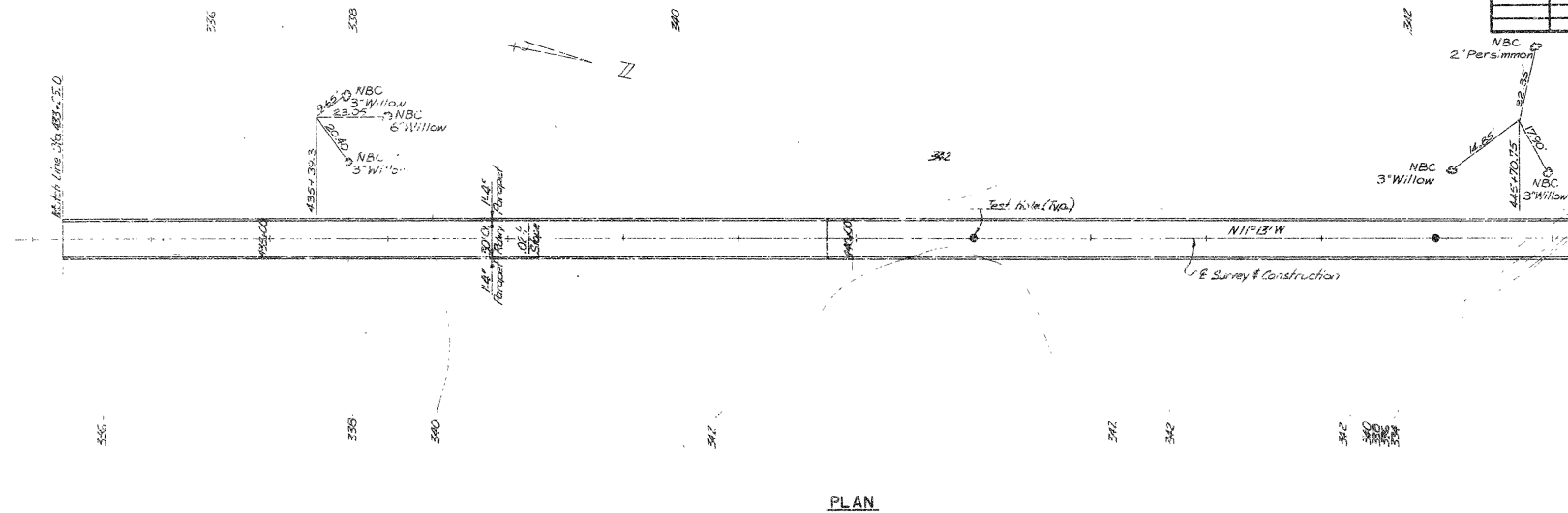
NOTE: For "BORING LEGEND" & "GENERAL NOTES" See Sheet 1, Orig. No. 18948

ALT 2
SHEET 2 OF 7
LAYOUT OF BRIDGE OVER ARKANSAS RIVER
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
LOGAN & JOHNSON CO.
ROUTE 109 SEC. 38.4
ARKANSAS STATE HIGHWAY COMMISSION

DESIGNED BY: K.M.G. DATE: 30 Dec. 73
CHECKED BY: M.E. DATE: 8/1/74
DESIGNED BY: E.T.F. DATE: 30 APR 73
BRIDGE ENGINEER
BRIDGE NO. 5600
DRAWING NO. 18949

FOR R/W DATA, SEE R/W PLANS

DATE REVISED	DATE FILMED	DATE REVISED	DATE FILMED	FILE NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				8	ARK.	RS-355(3)	25	64
				JOB NO.	155			
				S (1)	5600-Layout-18950			

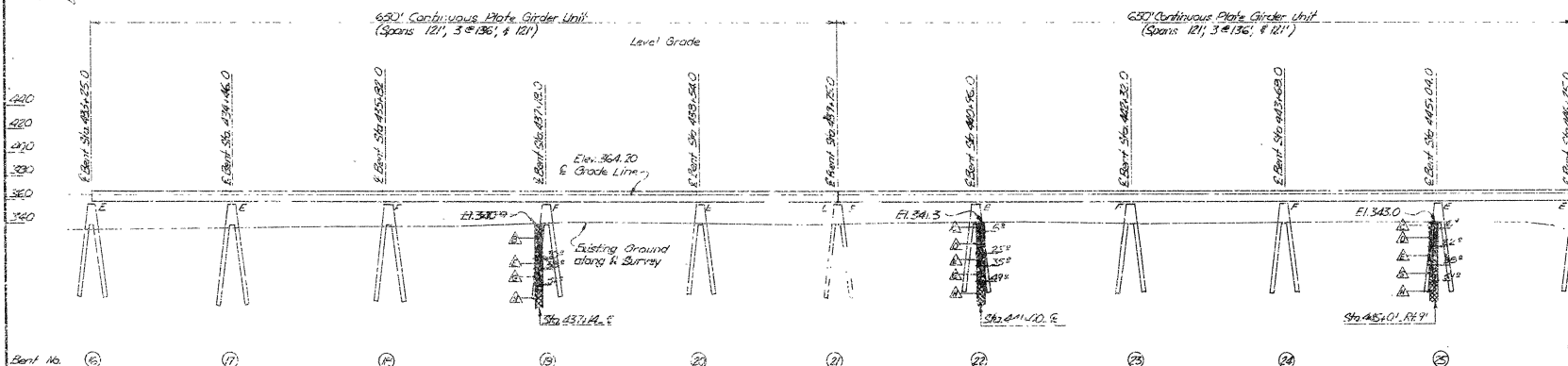


CAISSON LENGTH, EA. FT.

BENT NO.	CAISSON LENGTH (EA.) ALT. 2
16	60.0'
17	64.0'
18	62.0'
19	60.0'
20	59.0'
21	58.0'
22	58.0'
23	58.0'
24	58.0'
25	58.0'
26	58.0'

PLAN

TOTAL LENGTH OF BRIDGE - 651' 1"



ELEVATION

NOTE FOR BURNING LEGEND, SEE GENERAL NOTES;
See Sheet 1, Dwg No. 18948

ALT 2
SHEET 3 OF 7

LAYOUT OF BRIDGE OVER ARKANSAS RIVER
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
LOGAN & JOHNSON CO.
ROUTE 109 SEC. 3 & 4

ARKANSAS STATE HIGHWAY COMMISSION

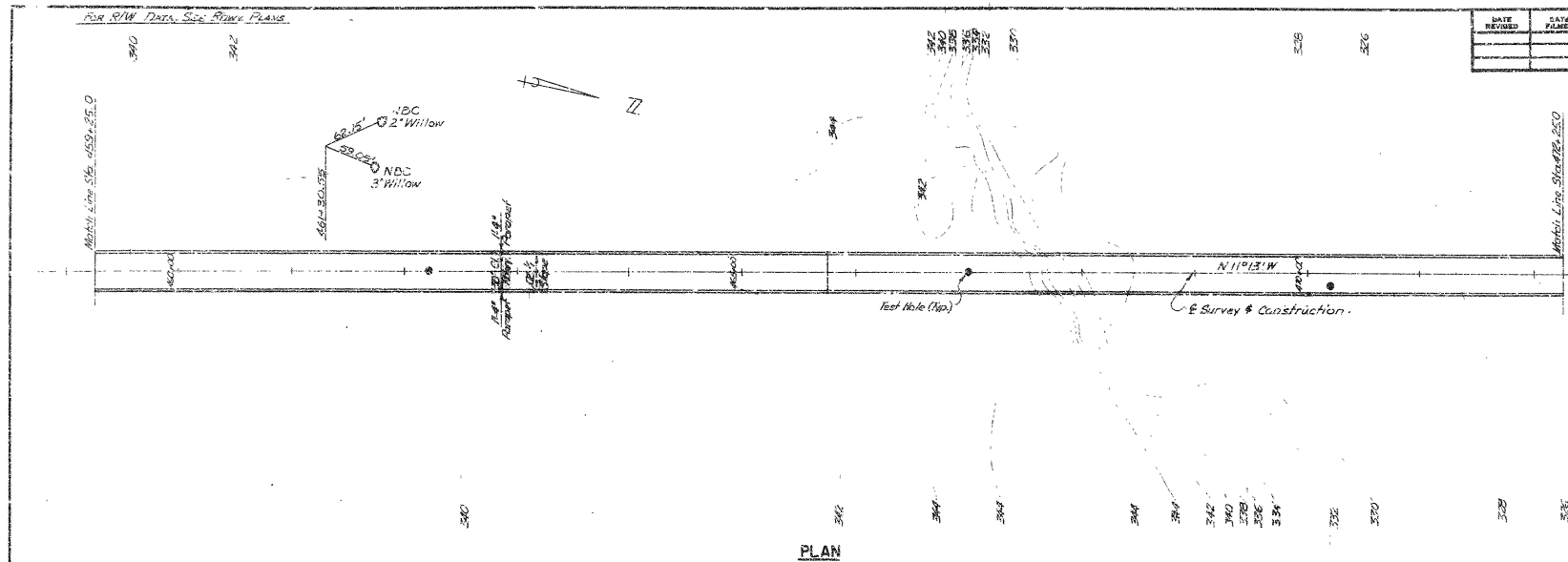
LITTLE ROCK, ARK.
DRAWN BY: K.M.G. DATE: 30 APR 73
CHECKED BY: UAS DATE: 21 AUG 74 SCALE: 1" = 50'-0"
DESIGNED BY: E.T. DATE: 30 APR 73
BRIDGE NO. 5600 DRAWING NO. 18950

DATE REVISION	DATE PLANNED	DATE REVISED	DATE FIELD	NO. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	SHEET NO.	TOTAL SHEETS
				6	ARK.	FS-383(3)	27	64

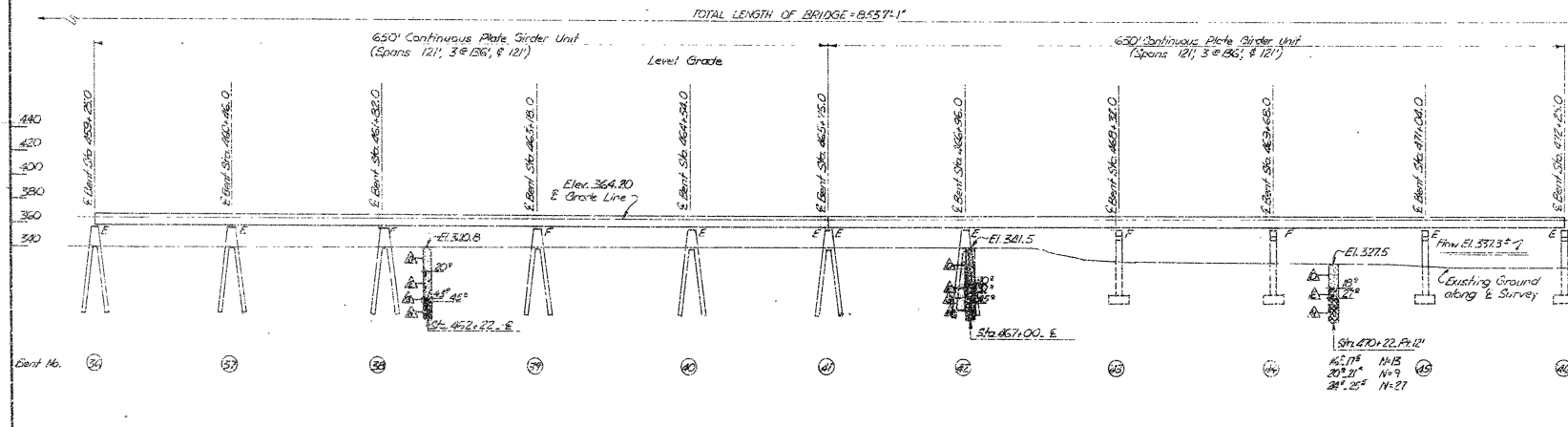
S ① 5600-Layout - 19952

CAISSON LENGTH - EA. FT.

BENT NO.	CAISSON LENGTH (EA.) - ALT. 2
36	54.0'
37	54.0'
38	54.0'
39	54.0'
40	54.0'
41	54.0'
42	54.0'



PLAN



ELEVATION

NOTE: BENTS 43 THRU 61, SHOWN ON ALT. 1 SHEETS 56, 57, ARE FOR REFERENCE ONLY IN CONTRACT 1 Job 1455.

NOTE: For "RUNING LEADING" & "GENERAL NOTES", see Sheet 1, Comp. No. 15,948.

ALT 2
SHEET 5 OF 7

LAYOUT OF BRIDGE OVER ARKANSAS RIVER
ARK. RIVER BR. & APPRS. (CLARKSVILLE)

LOGAN & JOHNSON CO.

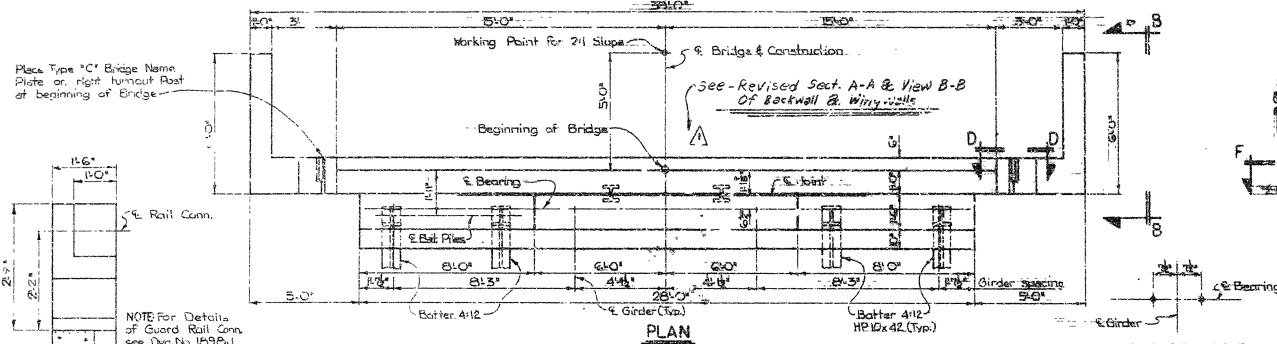
ROUTE 109 SEC. 38.4

ARKANSAS STATE HIGHWAY COMMISSION

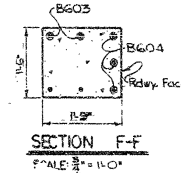
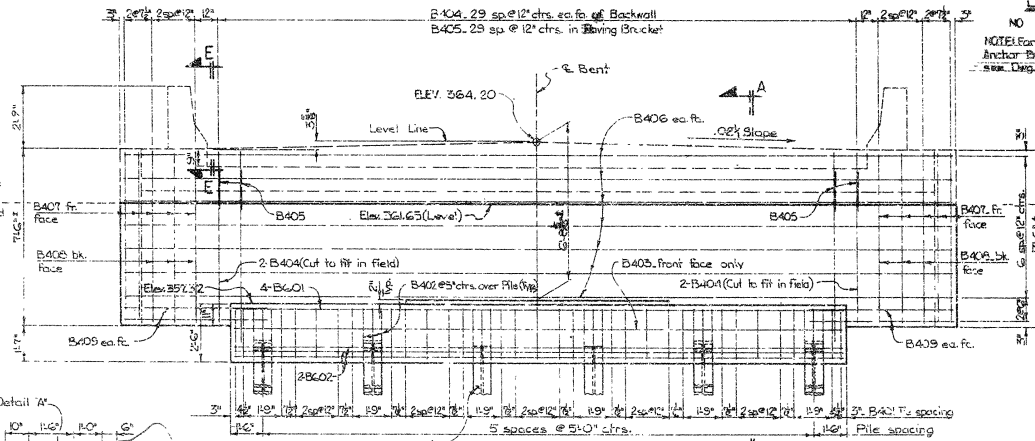
DESIGNED BY: K.A.G. DATE: 19 DEC 75
CHECKED BY: H.B. DATE: 21 AUG 76
DESIGNED BY: E.L.F. DATE: 30 APR 73
BRIDGE NO. 5600 DRAWING NO. 18952

403

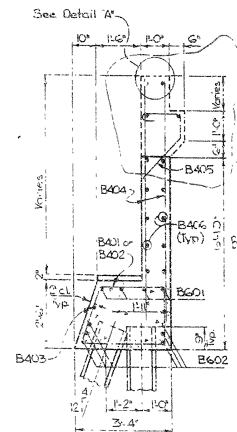
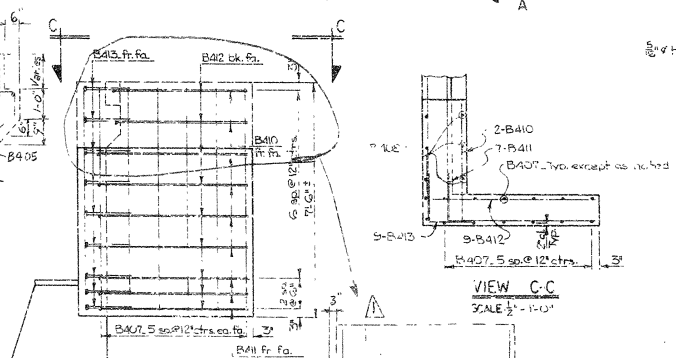
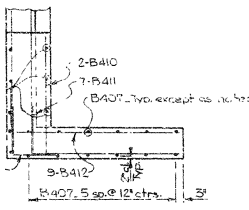
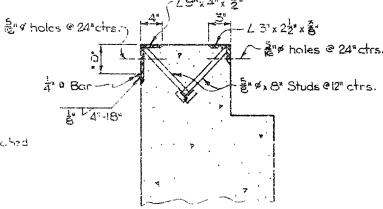
DATE	DATE	DATE	DATE	PRE. SOLD	STATE	FED. AID PROJ. NO.	SHEET	TOTAL
REV.	REV.	REV.	REV.	NO.			NO.	SHEETS
11-3-75				6	ARK.	RS-389(2)		
9-17-76	775-9-17-76			JOB NO.	1467	11	20	
S ① 5600 - BENT DTLS - 18953								

VIEW D-D
SCALE: 3/4" = 1'-0"ANCHOR BOLT
LAYOUT

NO SCALE
NOTE: For Details of
Anchor Bolt Spacing,
see Dwg. No. 18954

SECTION F-F
SCALE: 3/4" = 1'-0"VIEW E-E
SCALE: 3/4" = 1'-0"

ELEVATION

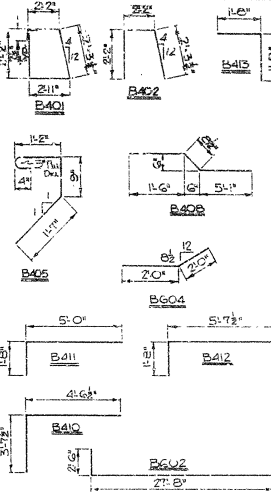
SECTION A-A
SCALE: 3/4" = 1'-0"VIEW B-B
SCALE: 3/4" = 1'-0"VIEW C-C
SCALE: 3/4" = 1'-0"DETAIL "A"
SCALE: 3/4" = 1'-0"

NOTE: The Dimension "D" shall conform to the
recommendations of the Seal Manufact-
urer as approved by the Bridge Engineer.
The depth of the Seal shall be approximately
equal to the uncompacted width of the Seal.

BAR LIST SUBSTR. CONTRACT

MARK	NO.	REQ'D	LENGTH	MIN DIA
B401	29		10'-2"	2"
B402	18		6'-6"	2"
B403	1		2'-0"	2"
B404	60		8'-1"	2"
B407	34		7'-2"	2"
B408	6		1'-3"	2"
B409	8		6'-6"	2"
B411	14		6'-7"	2"
B412	14		7'-3"	2"
B413	14		3'-3"	2"
B401	4		2'-2"	2"
B402	6		3'-2"	2"
B406	10		3'-8"	2"
B405	30		3'-11"	2"

BENDING DIAGRAM



BAR LIST SUPSTR. CONTRACT

MARK	NO. REQ'D	LENGTH	MIN DIA.
B406	4	3'-8"	5/8"
B410	4	8'-1"	2"
B412	4	7'-3"	2"
B413	4	3'-3"	2"
B414	12	1'-2"	5/8"
B403	8	4'-2"	5/8"
B404	6	3'-11"	3/8"

Dimensions are out to out of Bars.

For Details of Pile Splice and Pile Tip
Reinforcing, see Dwg. No. 18956

NOTE:

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1973
AND INTERIM SPECIFICATIONS.

LIVE LOAD: HS20

METHOD OF DESIGN: LOAD FACTOR

PIILING: HP10x42 PILING SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 55 TONS PER
PILE. PILES IN END BENTS SHALL BE DRIVEN AFTER EMBANKMENT TO SUBGRADE IS IN PLACE.
SEE DWG. NO. 18956 FOR DETAILS OF PILE TIP REINFORCING.

CONCRETE: ALL CONCRETE SHALL BE CLASS 5(AE) / MINIMUM 28 DAY COMPRESSIVE
STRENGTH $f_c = 3500$ PSI

CONSTRUCTION JOINTS SHALL BE HORIZONTAL OR AS SHOWN AND SHALL BE PROVIDED WITH KEYS
NOT LESS THAN THREE (3) INCHES HIGH COVERING THE MIDDLE ONE-THIRD OF BOTH DIMENSIONS.

REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615 OR A617
GRADE 60 (YIELD STRENGTH = 60,000 PSI)

FOR ADDITIONAL NOTES, SEE GENERAL NOTES, DWG. NO. 18941.

Revised-Changed Backwall & Wing walls As shown, 9-17-76 LDF

DETAILS OF END BENT NO. 1
ARK. RIVER BR. & APPRS. (CLARKSVILLE)

APPROACH BRIDGE SUPERSTRUCTURE

JOHNSON & LOGAN COUNTY
ROUTE 109 SEC. 3 & 4

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

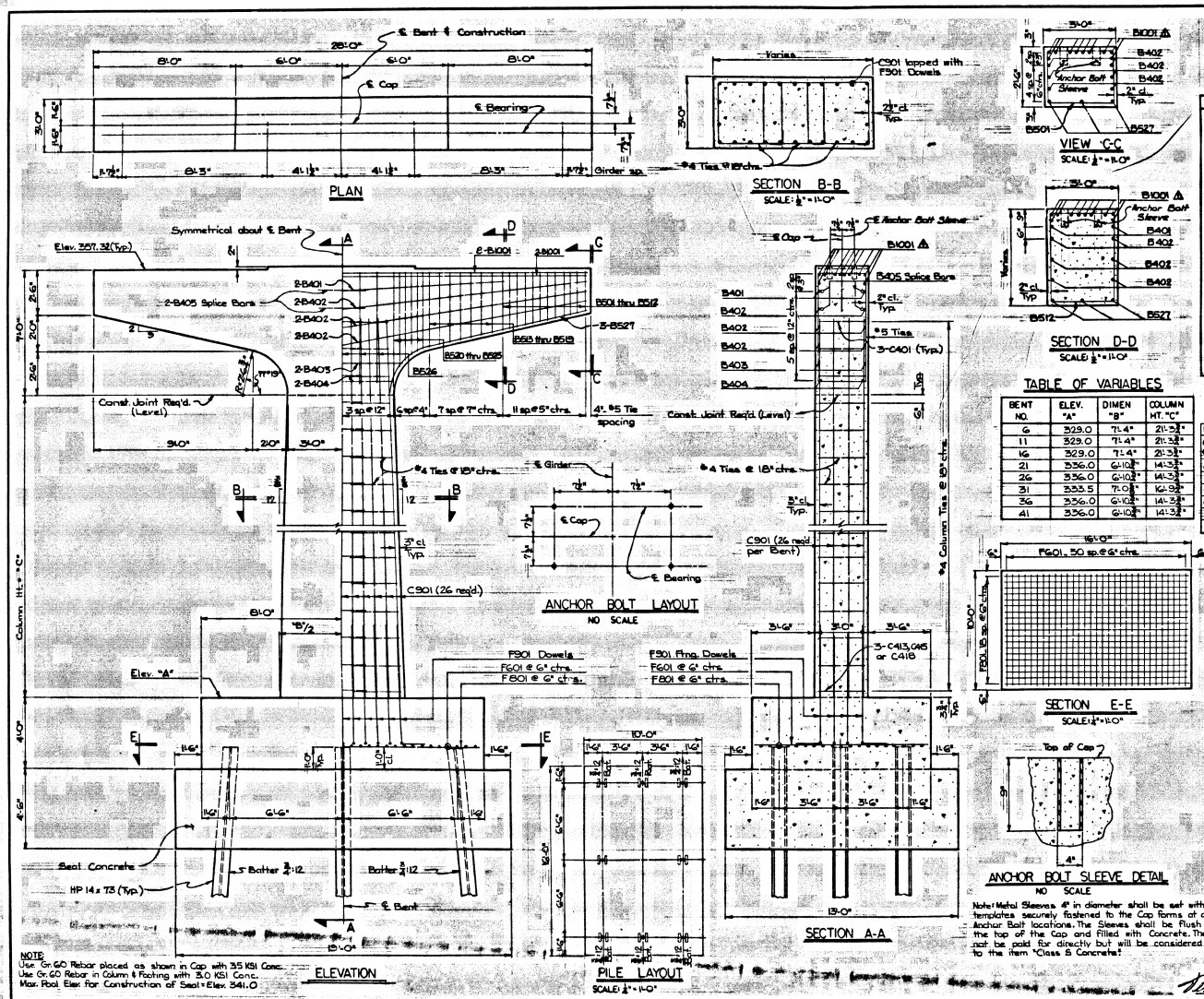
DESIGNED BY: K.M.G. DATE: 12 AUG 74

CHECKED BY: J.T.F. DATE: 16 AUG 74

BRIDGE NO. 5600 DRAWING NO. 18953

Updated for Job 149 - Contract 211

Steve Pinkerton
BRIDGE ENGINEER



DATE	BY	CHKD	APP'D	REV	DESCRIPTION
10 APR 75	51-4-2876				

ARK. RS-1895(3) 29 64
1855
50 BENT DTLS. 18954

BAR LIST COMMON BARS					
BENDING DIAGRAMS	MARK	NO.	REQ'D	LENGTH	PN DIA.
	B401	4	31.3'		5th
	B402	12	14.0'		5th
	B403	2	7.2'		5th
	B404	2	6.0'		5th
	B501 to B512	2 of each	Var 10'4" to 21'4"	2'-8"	2 1/2"
	B513 to B519	2 of each	Var 13'3" to 14'-8"	2'-8"	2 1/2"
	B520	2	14'-10"	4'-3"	2 1/2"
	B521	2	15'-2"	4'-4"	2 1/2"
	B522	2	15'-7"	4'-7"	2 1/2"
	B523	2	16'-1"	4'-10"	2 1/2"
	B524	2	16'-11"	5'-3"	2 1/2"
	B525	2	16'-1"	5'-10"	2 1/2"
	B526	5	14'-10"	6'-2"	2 1/2"
	B527	6	12'-11"	3'-10"	2 1/2"
	C301	26	1'-4 1/2"		5th
	F301	31	9'-5"		5th
	F302	19	15'-8"		5th
	F303	26	8'-5"	7'-0"	9"
	B1001	10	27'-8"		5th
	B405	4	2'-8"		5th

Dimensions are out to out of Bars.

BAR LIST VARIABLE BARS					
BENDING DIAGRAMS	MARK	NO.	REQ'D	LENGTH	PN DIA.
	C401 to C415	3 of 15 of 15 of each	Var 10'4" to 11'-9"	2'-7"	2"
	C416	3	11'-6"	2'-8"	2"
	C417	3	11'-7"	2'-9"	2"
	C418	3	11'-8"	2'-10"	2"
	C419	3	11'-9"	2'-11"	2"

Dimensions are out to out of Bars.

For Details of Pile Splice and Pile Tip Reinforcing, see Dwg. No. 18956.

For "GENERAL NOTES", see Dwg. No. 18954.

NOTES:

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1973 WITH 1974 INTERIM SPECIFICATIONS.

LIVE LOAD: HS20

METHOD OF DESIGN: LOAD FACTOR

PILING: HP14x73 PILING SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 96 TONS PER PILE. SEE DWG. NO. 18954 FOR DETAILS OF PILE TIP REINFORCING.

CONCRETE: ALL CONCRETE SHALL BE CLASS 5 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH $f'_c = 3000$ PSI FOR COLUMNS & FOOTINGS 3500 PSI FOR CAPS.

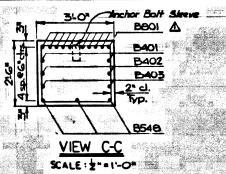
CONSTRUCTION JOINTS SHALL BE HORIZONTAL OR AS SHOWN & SHALL BE PROVIDED WITH KEYS NOT LESS THAN THREE (3) INCHES HIGH COVERING THE MIDDLE ONE-THIRD OF BOTH DIMENSIONS.

REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615, GRADE 60 (YIELD STRENGTH = 60,000 PSI).

FOR ADDITIONAL NOTES, SEE GENERAL NOTES DWG. NO. 18941.

REVISION LOCATION OF B1001: In Cap - 16 APR 75 - K.M.G.

ALTERNATE NO. 1
DETAILS OF EXP - EXP BENTS
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
MORRISON BLUFF - HWY. 194
GRADING & PARTIAL SUBSTR.
JOHNSON & LOGAN COUNTY
ROUTE 103 SEC. 3 & 4
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DESIGNED BY K.M.G. DATE 7 AUG 74
CHECKED BY J.E.B. DATE 15 MAY 74
DRAWN BY J.E.B. DATE 5 SEP 74
BRIDGE NO. 5600 DRAWING NO. 18954



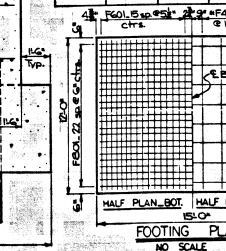
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NO	HT	HT	HT
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5	329.0	7-5 1/2	20-9 1/2
7	329.0	7-5 1/2	20-9 1/2
10	329.0	7-5 1/2	20-9 1/2
12	332.0	7-1 1/2	17-9 1/2
15	329.0	7-5 1/2	20-9 1/2
17	332.0	7-1 1/2	7-9 1/2
20	336.0	6-0 1/2	13-9 1/2
22	336.0	6-0 1/2	13-9 1/2
25	336.0	6-0 1/2	13-9 1/2
27	323.0	7-5 1/2	20-9 1/2
30	335.0	6-11 1/2	14-9 1/2
32	335.0	6-11 1/2	14-9 1/2
35	352.0	7-1 1/2	7-9 1/2
38	336.0	6-0 1/2	13-9 1/2
40	336.0	6-0 1/2	13-9 1/2
42	356.0	6-0 1/2	13-9 1/2

BAR LIST - COMMON BARS

MARK	NO. REQ'D	LENGTH	"A"	"B"	PIN DIA.	BENDING DIAGRAMS
B40K	4	13'-10"			5/8"	
B40Z	4	13'-11"			5/8"	
B403	4	14'-1"			5/8"	
B404	2	9'-6"			5/8"	
B405	2	6'-9"			5/8"	
B406	2	5'-11"			5/8"	
B501 to B503	2 of	Var. 10'-9" to 15'-0"	Var. 2'-3 1/2" to 3'-5 1/2"	2'-8"	2 1/2"	
B504	2 of	Var. 10'-8"	Var. 2'-2"	2'-8"	2 1/2"	
B524	each	to 2'-6"	to 3'-0 1/2"	2'-8"	2 1/2"	
B525 to B529	2 of	Var. 13'-9" to 14'-8 1/2"	Var. 3'-5 1/2" to 4'-0 1/2"	2'-8"	2 1/2"	
B530 to B534	each	Var. 13'-9" to 14'-0 1/2"	Var. 3'-5 1/2" to 3'-9 1/2"	2'-8"	2 1/2"	
B535	2	14'-7"	4'-10"	2'-8"	2 1/2"	
B536	2	14'-9"	4'-12"	2'-8"	2 1/2"	
B537	2	5'-2"	4'-5"	2'-8"	2 1/2"	
B538	2	5'-0"	4'-8"	2'-8"	2 1/2"	
B539	2	5'-6"	4'-10"	2'-8"	2 1/2"	
B540	2	14'-2"	3'-10 1/2"	2'-8"	2 1/2"	
B541	2	14'-4"	3'-11 1/2"	2'-8"	2 1/2"	
B542	2	14'-8"	4'-2"	2'-8"	2 1/2"	
B543	2	2'-4"	4'-8 1/2"	2'-8"	2 1/2"	
B544	2	16'-11"	4'-10 1/2"	2'-8"	2 1/2"	
B545	2	10'-0"	5'-9"	2'-8"	2 1/2"	
B546	2	11'-6"	5'-10 1/2"	2'-8"	2 1/2"	
B547	5	15'-3 1/2"	6'-4 1/2"	2'-8"	2 1/2"	
B548	6	12'-11"	3'-10"	9'-1"	2 1/2" R	
B501	11	7'-8"			5/8"	
B502	11	26'-0"			5/8"	
B503	26	4'-0", 4'-3"			5/8"	
F401	10	11'-8"			5/8"	
F402	8	14'-8"			5/8"	
F403	35	11'-8"			5/8"	
F601	25	14'-8"			5/8"	
F501	26	8'-5"	7'-0"	11'-8"	9"	
F502	4	2'-8"			5/8"	
F503	4	2'-8"			5/8"	
F504	4	2'-8"			5/8"	

Dimensions are to center of bars.

BAR LIST - VARIATION			
MARK	COLUMN		HT. "C"
	13: 9"	14: 9"	15: 9"
	NO. REQ'D.		
CA01 to CA12	3 of each	3 of each	3 of each
CA13	3	3	3
CA14			
CA15			
CA16			
CA17			



4-90
CITY

17' x 6' 10"

17' x 6' 10"

AN TOP

FOR "GENERAL NOTES", see Dwg. No. B8941

For Details of File Sillice and Tie Tip Reinforcing, see Dwg. No. B8956

△ REINFORCED CONCRETE OF BSCH 18002 IN CAP. 16 ARE. TS - K.M.G.

ALTERNATE NO.1

DETAILS OF EXP. INT. BENTS

ARK. RIVER BR. & APPRS. (CLARKSVILLE)

MORRISON BLUFF - HWY. 194

GRADING & PARTIAL SUBSTR.

SUBSTR. & LOCAL CONC.

ROUTE 109 SECT. 3&4

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: K.M.G. DATE: AUG. 28

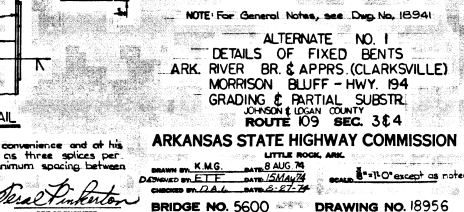
CHECKED BY: E.T.F. DATE: MAY 24

DESIGNED BY: J.D.A. DATE: DEC. 14

BRIDGE NO. 5600 DRAWING NO. B8955

SCALE: SEE PLAN except as noted

Steve P. Benton



DRAWN BY: K.M.G. DATE: 8 AUG 74
DESIGNED BY: ETF DATE: 15 MAY 74 SCALE: 1"=10' except as noted
CHECKED BY: DAL DATE: 8-27-74
BRIDGE NO. 5600 DRAWING NO. 18956

MARK	NO.	REQD.	LENGTH	"A"	"B"	PN	DA
B401	2	2" x 3"				5tr.	
B402	3	12" x 6"				5tr.	
B403	4	12" x 6"				5tr.	
B404	4	12" x 6"				5tr.	
B405	4	each	Net 39" to 13.5"	Net 24.5" to 4.4"	2 11/16"	15.5tr.	
B416	4	13.5" x 4.4"			2 11/16"	15.5tr.	
B501	1	12"10" x 3.5"			2.5"	2"	
B502	2	12"11" x 3.5" x 4"			2.5"	2 1/2"	
B503	2	13.5" x 3.5"			2.5"	2 1/2"	
B504	2	15.5" x 5.5"			2.5"	2 1/2"	
B505	2	15.4" x 5.6"			2.8"	2 1/2"	
B506	2	15.7" x 5.7"			2.8"	2 1/2"	
B507	2	15.8" x 5.8"			2.8"	2 1/2"	
B508	2	14" x 4"			2.8"	2 1/2"	
B509	2	14.0" x 4.0"			2.8"	2 1/2"	
B510	2	14.5" x 4.2"			2.8"	2 1/2"	
B511	2	15.1" x 4.3"			2.8"	2 1/2"	
B512	2	15.2" x 4.3"			2.8"	2 1/2"	
B513	2	16.1" x 4.0"			2.8"	2 1/2"	
B514	2	16.7" x 5.1"			2.8"	2 1/2"	
B515	2	17.2" x 5.4"			2.8"	2 1/2"	
B601	12	17.5" x 2"				5tr.	
B701	9	27" x 0"				5tr.	
B801	16	71"10" x 2.4"			6"	5tr.	
B901	11	24" x 5"	71"10"	2.4"	5"	24.5tr.	
C401	4	10"10"				Circle	
C001	44	17" x 2"				5tr.	
C1002	#44	6" x 0"				5tr.	

"A"

B901

24"

Dimensions are out to out of Bore

For 'Prestressed Cylinder Pile' notes see Dwg. No. 18958

NOTES

NOTES

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1973 WITH 1974 INTERIM SPECIFICATIONS.

LIVE LOAD: HS20

METHOD OF DESIGN: LOAD FACTOR

CONCRETE: ALL CONCRETE SHALL BE CLASS 5 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH $f_c = 3000$ PSI FOR DRILLED CAISSON.
= 3500 PSI FOR CYLINDER PILE ALTERNATE & CONCRETE ABOVE ELEV. 340.0

CONSTRUCTION JOINTS SHALL BE HORIZONTAL, OR AS SHOWN AND SHALL BE PROVIDED WITH KEYS NOT LESS THAN THREE INCHES HIGH COVERING THE MIDDLE ONE-THIRD OF BOTH DIMENSIONS.

REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615,
GRADE 60 (YIELD STRENGTH = 60,000 PSI)

ALTERNATE NO. 2
DETAILS OF EXP - EXP BENTS
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
MORRISON BLUFF - HWY. 194
GRADING & PARTIAL SUBSTR.
JOHNSON & LOGAN COUNTY
ROUTE 109 SEC. 3 & 4

ARKANSAS STATE HIGHWAY COMMISSION

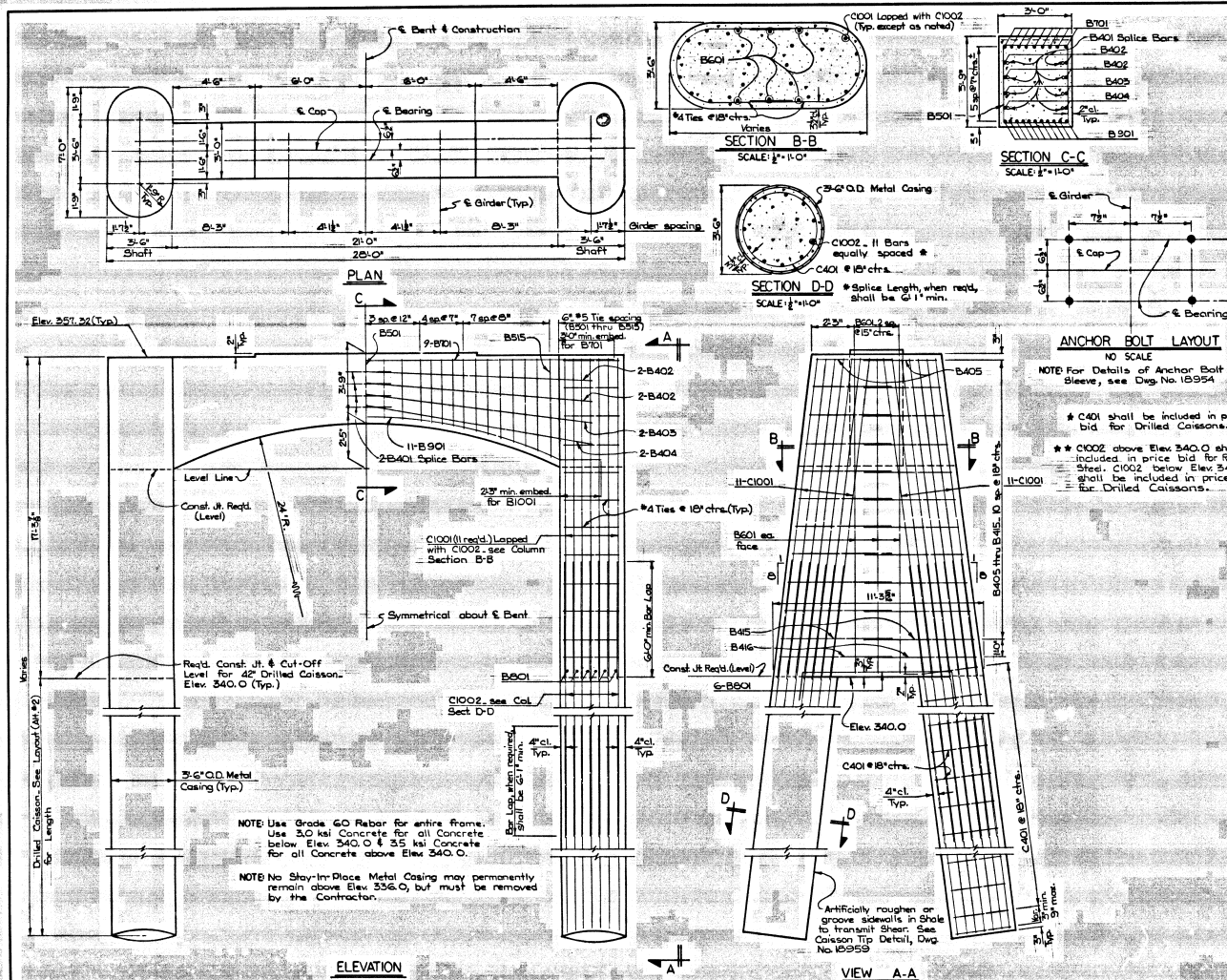
LITTLE ROCK, ARK.

DRAWN BY: K.M.G. DATE: 22 AUG 74

DESIGN BY: E.F. DATE: 16 AUG 74

SCALE: $\frac{1}{8} = 1'-0"$ except as noted

BRIDGE NO. 5600 DRAWING NO. 18957



MARK	NO. REQ'D.	LENGTH	A	B	PN. DIA.	BENDING	DIAGRAMS
B401	4	2'-8"			5/8"		
B402	8	12'-6"			5/8"		
B403	4	12'-6"			5/8"		
B404	4	12'-6"			9/16"		
B405	1	4'-0"	10'-2 1/2"	10'-2 1/2"	1/2" R		
B415	each	10'-3 1/2"	10'-2 1/2"	10'-2 1/2"	1/2" R		
B501	4	12'-10"	3'-5 1/2"	2'-5"	2 1/2"		
B502	4	12'-11"	3'-5 1/2"	2'-5"	2 1/2"		
B503	4	13'-0"	3'-5 1/2"	2'-5"	2 1/2"		
B504	4	13'-4"	3'-5 1/2"	2'-5"	2 1/2"		
B505	4	13'-7"	3'-5 1/2"	2'-5"	2 1/2"		
B506	4	13'-11"	3'-5 1/2"	2'-5"	2 1/2"		
B507	4	14'-0"	3'-5 1/2"	2'-5"	2 1/2"		
B508	4	14'-3"	3'-5 1/2"	2'-5"	2 1/2"		
B509	4	15'-1"	3'-5 1/2"	2'-5"	2 1/2"		
B510	4	15'-8"	4'-0"	2'-5"	2 1/2"		
B511	4	16'-0"	5'-0 1/2"	2'-5"	2 1/2"		
B512	4	17'-1"	5'-5"	2'-5"	2 1/2"		
B601	12	16'-0"			5/8"		
B701	12	20'-0"			5/8"		
B801	24	12'-6"	7'-10"	2'-4 1/2"	6"		
B802	12	26'-0"	21'-10"	2'-3 1/2"	4 1/2" R		
C001	48	17'-11"			3/8"		
C1001	48	17'-11"			3/8"		
C1002	48	6'-0"			3/8"		

★★ C1002 above Elev. 340.0 shall be included in price bid per pound for Reinforcing Steel. C1002 below Elev. 340.0 shall be included in price bid for Drilled Caissons.

For additional Notes see Dwg. No. 18957

For "Prestressed Cylinder Pile" notes see Dwg. No. 18958

ARKANSAS STATE HIGHWAY COMMISSION

LITTLE ROCK, ARK.

DRAWN BY: K.M.G. DATE: 20 AUG. 74

DESIGN BY: E.F. DATE: 16 AUG. 74

CHECKED BY: J.H. DATE: 2-27-74

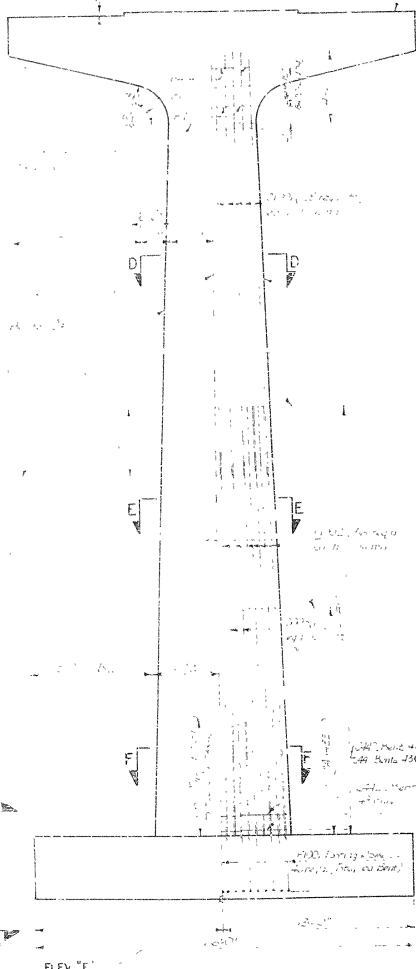
SCALE: 1"=10'-0" except as noted

BRIDGE NO. 5600 DRAWING NO. 18959

BRIDGE NO. 5600 DRAWING NO. 18959

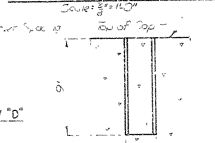


PLAN
Scale: 1"=10'



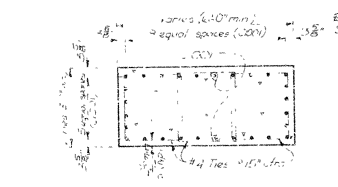
ELEVATION
Scale: 1"=10'

TYP ANCHOR BOLT LAYOUT
Scale: 1"=10'

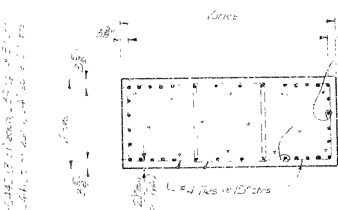


ANCHOR BOLT SLEEVE DETAIL
No Scale

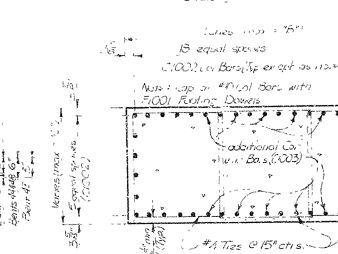
NOTE: Min. 4" diameter shall be used with lap joints. Sleeves shall be filled with concrete and filled with concrete. They will not be used for directly but will be considered as part of the main structure.



SECTION D-D
Scale: 1"=10'



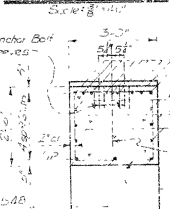
SECTION E-E
Scale: 1"=10'



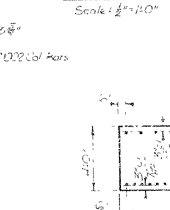
SECTION F-F
Scale: 1"=10'



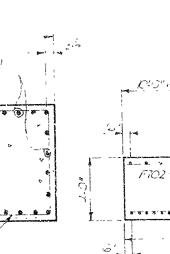
HALF-CAP ELEVATION
Scale: 1"=10'



VIEW B-B
Scale: 1"=10'



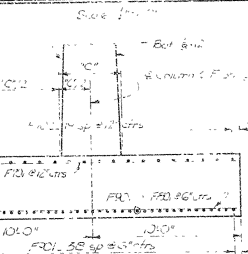
SECTION C-C
Scale: 1"=10'



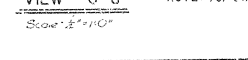
SECTION G-G
Scale: 1"=10'



TRANSVERSE FOOTING ELEVATION
Scale: 1"=10'



VIEW G-G
Scale: 1"=10'



NOTE: For Circular Footing Option - See Draw. 1925B

DATE REVISION	BY	NO.	REVISION	DATE

5600 BENT DTLS. 1925C

BAR LIST (EA BENT) COMMON BARS

BENDING DIAGRAMS	MARK	NO.	REQ'D	LENGTH	A	B	FIN. DIA.
	1	1	1	10.0'	10.0'	10.0'	10.0'
	2	2	2	10.0'	10.0'	10.0'	10.0'
	3	3	3	10.0'	10.0'	10.0'	10.0'
	4	4	4	10.0'	10.0'	10.0'	10.0'
	5	5	5	10.0'	10.0'	10.0'	10.0'
	6	6	6	10.0'	10.0'	10.0'	10.0'
	7	7	7	10.0'	10.0'	10.0'	10.0'
	8	8	8	10.0'	10.0'	10.0'	10.0'
	9	9	9	10.0'	10.0'	10.0'	10.0'
	10	10	10	10.0'	10.0'	10.0'	10.0'

BAR LIST VARIABLE BARS

MARK	NO.	REQ'D	LENGTH	"X"	"Y"	FIN. DIA.	BENDING DIAGRAMS
1	1	1	10.0'	10.0'	10.0'	10.0'	
2	2	2	10.0'	10.0'	10.0'	10.0'	
3	3	3	10.0'	10.0'	10.0'	10.0'	
4	4	4	10.0'	10.0'	10.0'	10.0'	
5	5	5	10.0'	10.0'	10.0'	10.0'	
6	6	6	10.0'	10.0'	10.0'	10.0'	
7	7	7	10.0'	10.0'	10.0'	10.0'	
8	8	8	10.0'	10.0'	10.0'	10.0'	
9	9	9	10.0'	10.0'	10.0'	10.0'	
10	10	10	10.0'	10.0'	10.0'	10.0'	

TABLE OF VARIABLES

	"A"	"B"	"C"	ELEV. "D"	ELEV. "E"
FOOTING	10.0'	10.0'	10.0'	10.0'	10.0'
PIER	10.0'	10.0'	10.0'	10.0'	10.0'
ABUTMENT	10.0'	10.0'	10.0'	10.0'	10.0'
BRIDGE	10.0'	10.0'	10.0'	10.0'	10.0'

DESIGN SPECIFICATIONS - AASHTO STANDARD SPECIFICATIONS FOR BRIDGES AND STRUCTURES, 1975 WITH 1974 INTERIM AMENDMENTS.

CONCRETE - ALL CONCRETE SHALL BE CLASS 4000 (MINIMUM 28 DAY COMPRESSIVE STRENGTH $f_c = 4000$ PSI).

REINFORCING STEEL - REINFORCING STEEL SHALL CONFORM TO ASTM SPECIFICATION A615 GRADE 60 (YIELD STRENGTH = 60,000 PSI).

CONSTRUCTION JOINTS - CONSTRUCTION JOINTS SHALL BE PROVIDED WITH SPLICING PLATES OR BOLT SPICES. BOLT SPICES SHALL BE PROVIDED WITH RIVETS NOT LESS THAN 1/4" DIA. HIGH CARBON STEEL RIVETS OF BOTH DIRECTIONS.

SPLICING OF COLUMN BARS - BOLT SPICES MAY BE USED INSTEAD OF LAPED SPLICING. IF BOLT SPICES ARE USED THEY SHALL CONFORM TO A615 GRADE 60 (YIELD STRENGTH = 60,000 PSI).

REINFORCING STEEL - BOLT SPICES SHALL BE PROVIDED WITH RIVETS NOT LESS THAN 1/4" DIA. HIGH CARBON STEEL RIVETS OF BOTH DIRECTIONS.

FOR ADDITIONAL NOTES, SEE GENERAL NOTES FOR DRAWING 1925C.

FOUNDATION PRESSURE GROUP LOAD MAX. MIN. 6.9 KSF 0.30 KSF

DETAILS OF FIXED BENTS 43, 44, 48 & 49 ARK. RIVER BR. & APPRS. (CLARKSVILLE) MAIN BRIDGE SUBSTRUCTURE LOGAN & JOHNSON CO.

ROUTE 109 SEC. 3 & 4 ARKANSAS STATE HIGHWAY COMMISSION LITTLE ROCK, ARK. DRAWN BY K.M.G. DATE 2 APR 75 CHECKED BY DATE DESIGNED BY DATE BRIDGE NO. 5600 DRAWING NO. 1925C

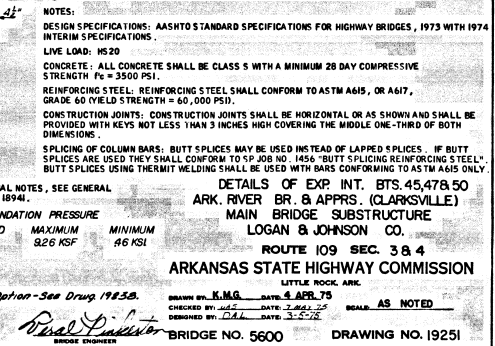
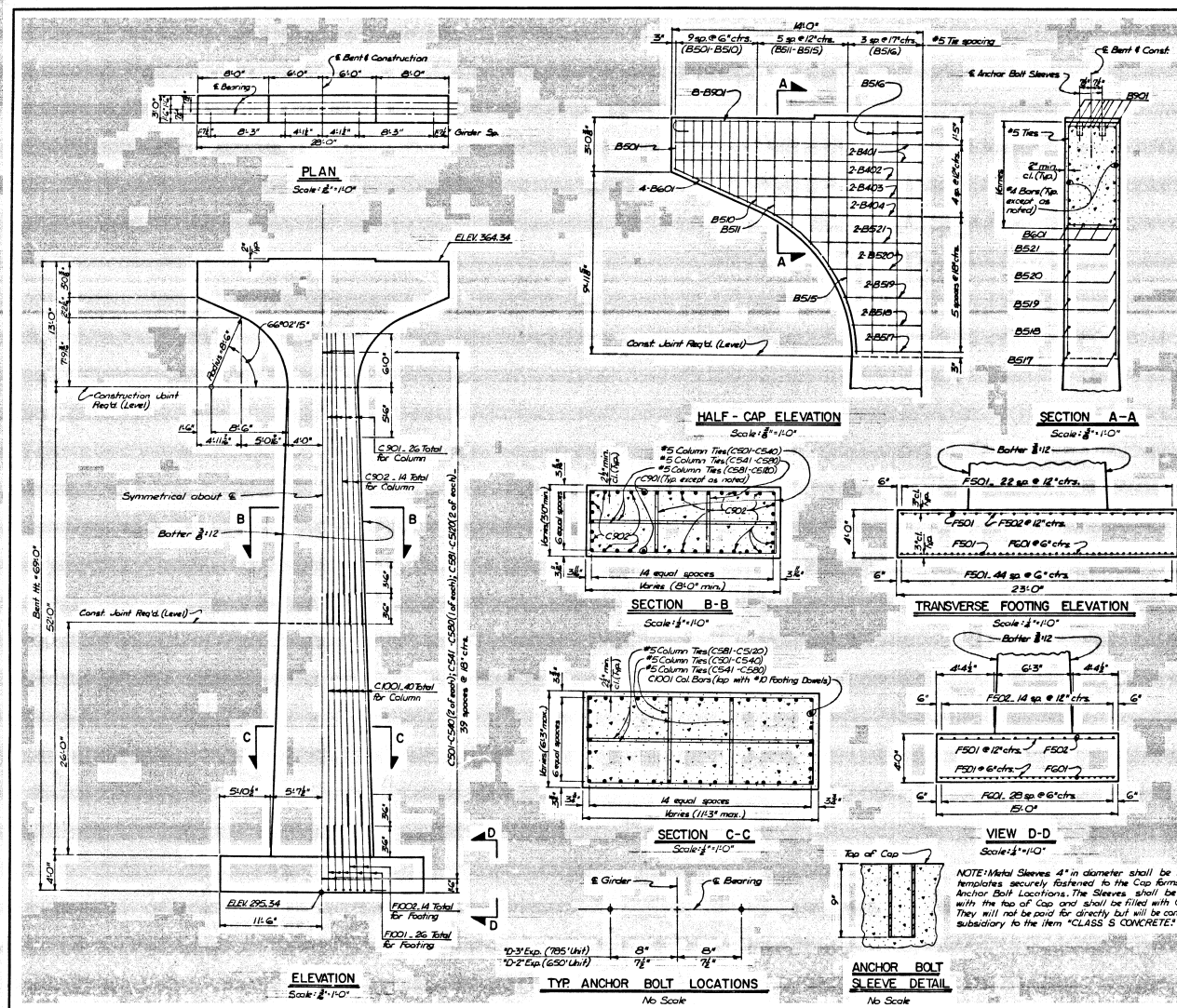


Figure 1. The effect of the concentration of the inhibitor on the rate of polymerization of the monomer.



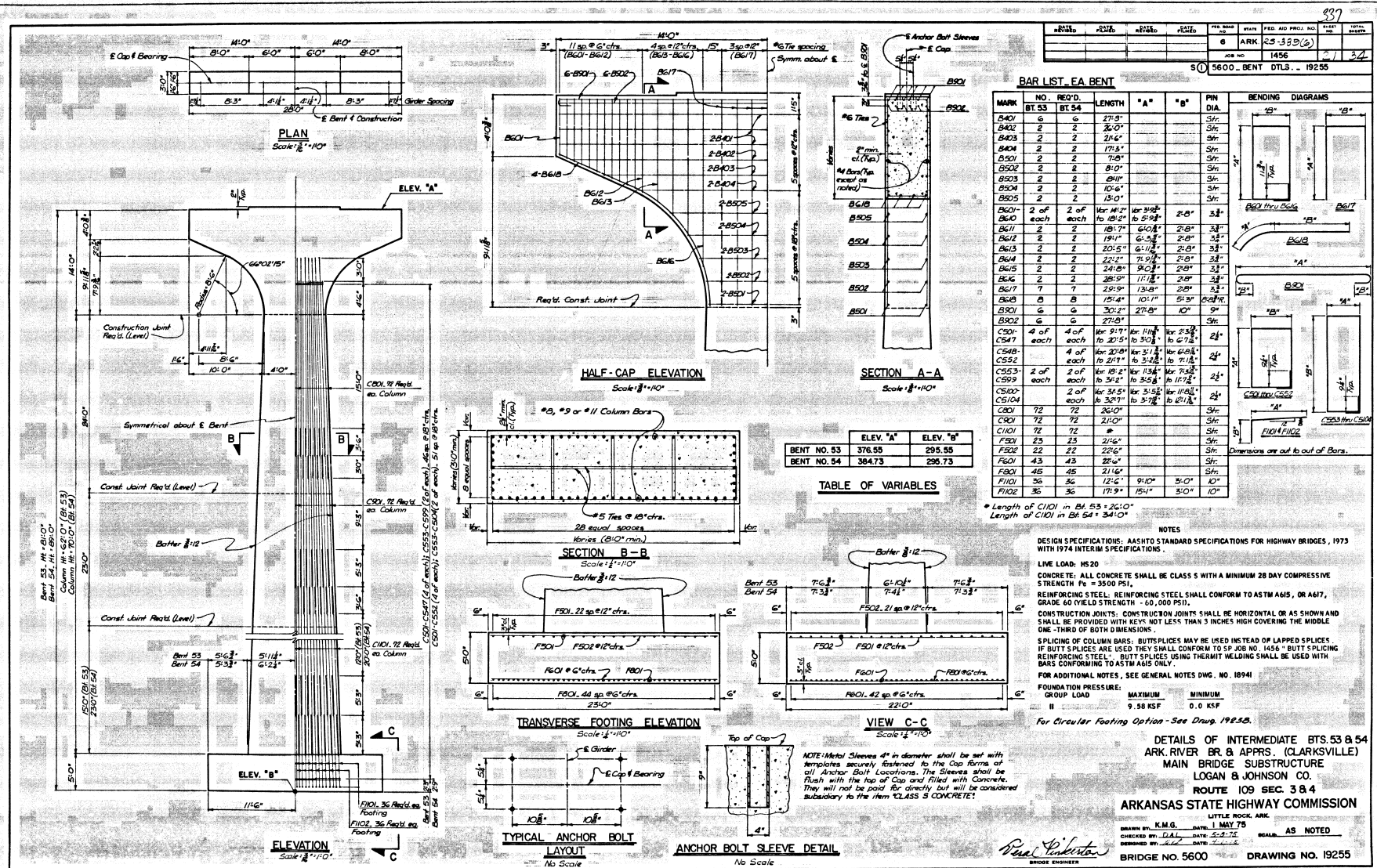
DATE	BY	CHKD	APP'D	PROJ. NO.	DATE	BY	CHKD	APP'D
10/1/73	K.M.G.			5600	10/1/73	K.M.G.		
				5600 - BENT DTLS. - 19253				

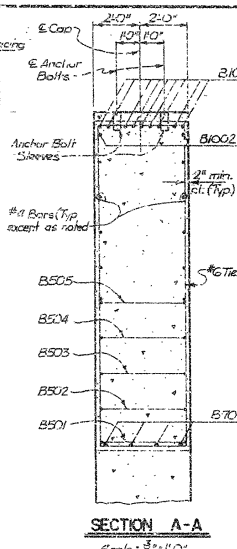
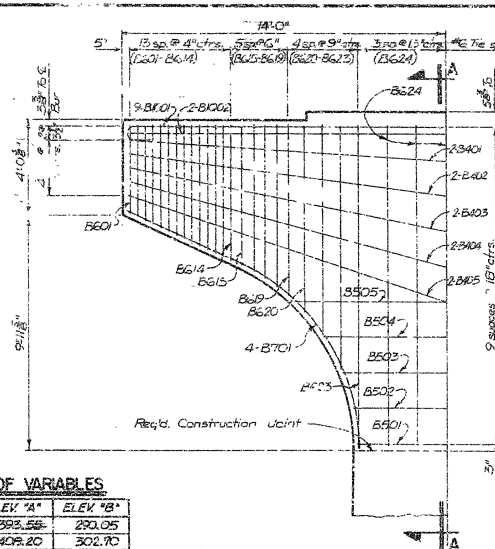
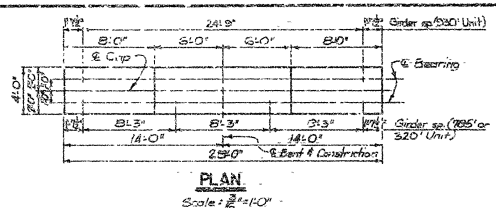
MARK	NO.	REQ'D	LENGTH	"A"	"B"	PIN DIA.	BENDING DIAGRAMS
B401	4	27'-0"			3'-0"		
B402	2	26'-0"			3'-0"		
B403	2	21'-0"			3'-0"		
B404	2	17'-0"			3'-0"		
B501	2	27'-0"	for 11'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B511	2	16'-0"	5'-0"	2'-0"	2 1/2"		
B512	2	18'-0"	5'-0"	2'-0"	2 1/2"		
B513	2	20'-0"	6'-0"	2'-0"	2 1/2"		
B514	2	22'-0"	6'-0"	2'-0"	2 1/2"		
B515	2	18'-0"	5'-0"	2'-0"	2 1/2"		
B516	5	27'-0"	12'-0"	2'-0"	2 1/2"		
B517	2	7'-0"			3'-0"		
B518	2	7'-0"			3'-0"		
B519	2	8'-0"			3'-0"		
B520	2	10'-0"			3'-0"		
B521	2	12'-0"			3'-0"		
B522	2	15'-0"	12'-0"	5'-0"	3'-0"		
B523	2	30'-0"	10'-0"	9"			
B524	2	27'-0"			3'-0"		
B525	2	27'-0"			3'-0"		
B526	2	27'-0"	for 11'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B527	1	of	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B528	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B529	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B530	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B531	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B532	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B533	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B534	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B535	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B536	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B537	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B538	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B539	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B540	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B541	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B542	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B543	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B544	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B545	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B546	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B547	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B548	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B549	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B550	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B551	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B552	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B553	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B554	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B555	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B556	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B557	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B558	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B559	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B560	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B561	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B562	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B563	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B564	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B565	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B566	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B567	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B568	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B569	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B570	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B571	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B572	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B573	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B574	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B575	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B576	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B577	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B578	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B579	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B580	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B581	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B582	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B583	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B584	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B585	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B586	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B587	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B588	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B589	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B590	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B591	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B592	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B593	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B594	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B595	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B596	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B597	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B598	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B599	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B600	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B601	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B602	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B603	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B604	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B605	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B606	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B607	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B608	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B609	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B610	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B611	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B612	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B613	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B614	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B615	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B616	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B617	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B618	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B619	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B620	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B621	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B622	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B623	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B624	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B625	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	
B626	2	27'-0"	for 10'-0" to 21'-0"	5'-0"	2'-0"	2 1/2"	

DATE	BY	CHKD	DATE	FILED	PROJECT NO.	WORK NO.	DESIGN NO.	REVISION NO.	REVISION
11/1/77	11/1/77				5	ARK	25-389(6)		
					JOB NO.		1456	20	34
							5600 - RENT DTLS - 19254		

BAR LIST

MARK	NO.	REQ'D	LENGTH	"A"	"B"	FIN DIA	BENDING DIAGRAMS
B401	2	27'-0"				5/8"	
B402	2	26'-0"				5/8"	
B403	2	24'-6"				5/8"	
B404	2	24'-6"				5/8"	
B501	2	7'-2"				5/8"	
B502	2	7'-11"				5/8"	
B503	2	8'-5"				5/8"	
B504	2	10'-4"				5/8"	
B505	2	12'-0"				5/8"	
B506	2	12'-0"				5/8"	
B507	2	12'-0"				5/8"	
B508	2	12'-0"				5/8"	
B509	2	12'-0"				5/8"	
B510	2	12'-0"				5/8"	
B511	2	12'-0"				5/8"	
B512	2	12'-0"				5/8"	
B513	2	12'-0"				5/8"	
B514	2	12'-0"				5/8"	
B515	2	12'-0"				5/8"	
B516	2	12'-0"				5/8"	
B517	2	12'-0"				5/8"	
B518	2	12'-0"				5/8"	
B519	2	12'-0"				5/8"	
B520	2	12'-0"				5/8"	
B521	2	12'-0"				5/8"	
B522	2	12'-0"				5/8"	
B523	2	12'-0"				5/8"	
B524	2	12'-0"				5/8"	
B525	2	12'-0"				5/8"	
B526	2	12'-0"				5/8"	
B527	2	12'-0"				5/8"	
B528	2	12'-0"				5/8"	
B529	2	12'-0"				5/8"	
B530	2	12'-0"				5/8"	
B531	2	12'-0"				5/8"	
B532	2	12'-0"				5/8"	
B533	2	12'-0"				5/8"	
B534	2	12'-0"				5/8"	
B535	2	12'-0"				5/8"	
B536	2	12'-0"				5/8"	
B537	2	12'-0"				5/8"	
B538	2	12'-0"				5/8"	
B539	2	12'-0"				5/8"	
B540	2	12'-0"				5/8"	
B541	2	12'-0"				5/8"	
B542	2	12'-0"				5/8"	
B543	2	12'-0"				5/8"	
B544	2	12'-0"				5/8"	
B545	2	12'-0"				5/8"	
B546	2	12'-0"				5/8"	
B547	2	12'-0"				5/8"	
B548	2	12'-0"				5/8"	
B549	2	12'-0"				5/8"	
B550	2	12'-0"				5/8"	
B551	2	12'-0"				5/8"	
B552	2	12'-0"				5/8"	
B553	2	12'-0"				5/8"	
B554	2	12'-0"				5/8"	
B555	2	12'-0"				5/8"	
B556	2	12'-0"				5/8"	
B557	2	12'-0"				5/8"	
B558	2	12'-0"				5/8"	
B559	2	12'-0"				5/8"	
B560	2	12'-0"				5/8"	
B561	2	12'-0"				5/8"	
B562	2	12'-0"				5/8"	
B563	2	12'-0"				5/8"	
B564	2	12'-0"				5/8"	
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B666	2	12'-0"				5/8"	
B667	2	12'-0"				5/8"	
B668	2	12'-0"				5/8"	
B669	2	12'-0"				5/8"	
B670	2	12'-0"				5/8"	
B671	2	12'-0"				5/8"	
B672	2	12'-0"				5/8"	
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B676	2	12'-0"				5/8"	
B677	2	12'-0"				5/8"	
B678	2	12'-0"				5/8"	
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B680	2	12'-0"				5/8"	
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B682	2	12'-0"				5/8"	
B683	2	12'-0"				5/8"	
B684	2	12'-0"				5/8"	
B685	2	12'-0"				5/8"	
B686	2	12'-0"				5/8"	
B687	2	12'-0"				5/8"	
B688	2	12'-0"				5/8"	
B689	2	12'-0"				5/8"	
B690	2	12'-0"				5/8"	
B691	2	12'-0"				5/8"	
B692	2	12'-0"				5/8"	
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B710	2	12'-0"				5/8"	
B711	2	12'-0"				5/8"	
B712	2	12'-0"				5/8"	
B713	2	12'-0"				5/8"	
B714	2	12'-0"				5/8"	
B715	2	12'-0"				5/8"	
B716	2	12'-0"				5/8"	
B717	2	12'-0"				5/8"	
B718	2	12'-0"				5/8"	
B719	2	12'-0"				5/8"	
B720	2	12'-0"				5/8"	
B721	2	12'-0"				5/8"	
B722	2	12'-0"				5/8"	
B723	2	12'-0"				5/8"	
B724	2	12'-0"				5/8"	
B725	2	12'-0"				5/8"	
B726	2	12'-0"				5/8"	
B727	2	12'-0"				5/8"	
B728	2	12'-0"				5/8"	
B729	2	12'-0"				5/8"	
B730	2	12'-0"				5/8"	</





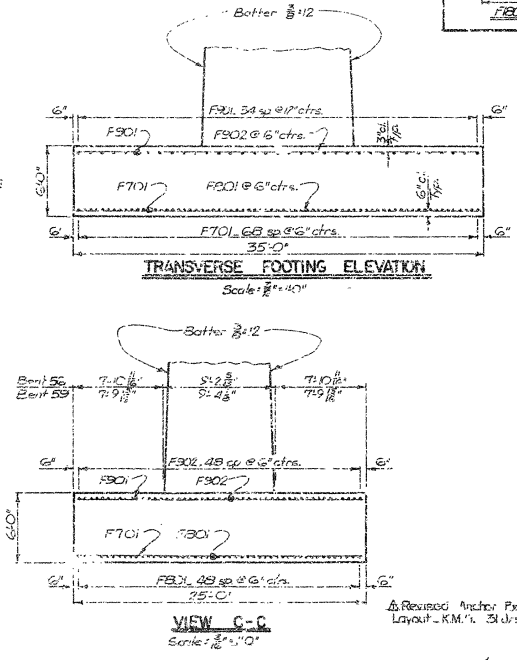
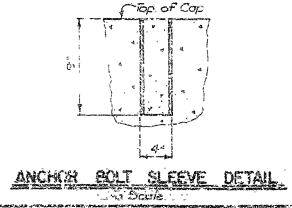
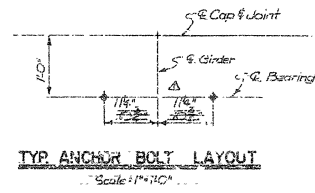
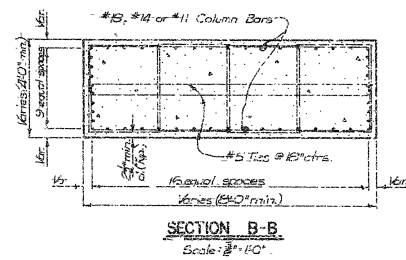
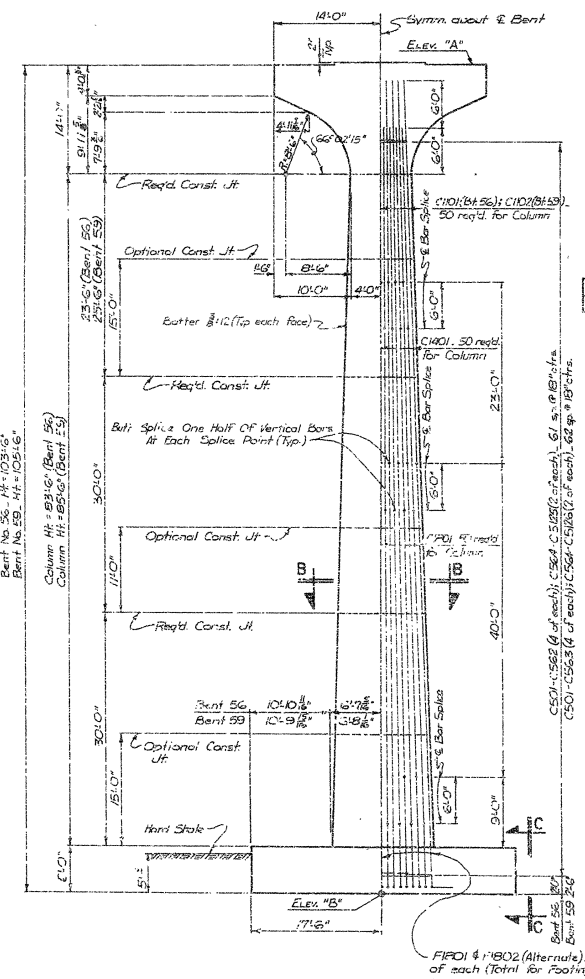
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6
APR. 69-282(6)
JOB NO. 185
23 32
S(1) 5600-5621 DT 5-19257

BAR LIST						
MARK	NO. REQ'D	LENGTH	"A"	"B"	FIN DIA.	
B401	4 of each	16'-13 1/2"	16'-13 1/2"	3'-6"	3/8"	
B405	4 of each	16'-13 1/2"	16'-13 1/2"	3'-6"	3/8"	
B501	1	23'-5"	7'-8"	3'-6"	3/8"	
B502	1	24'-1"	8'-0"	3'-6"	3/8"	
B503	1	25'-11"	8'-11"	3'-6"	3/8"	
B504	1	28'-11"	10'-5"	3'-6"	3/8"	
B505	1	33'-11"	12'-1"	3'-6"	3/8"	
B601	2 of each	16'-13 1/2"	16'-13 1/2"	3'-6"	3/8"	
B604	2 of each	16'-13 1/2"	16'-13 1/2"	3'-6"	3/8"	
B605	2	20'-7"	2'-0"	3'-6"	3/8"	
B701	8	15'-3"	15'-3"	3'-1"	3/8"	
B800	4	20'-0"	21'-8"	0'-11 1/2"	10"	
B802	2	27'-8"	27'-8"	0'-11 1/2"	10"	
C501	4 of each	16'-11 1/2"	16'-3 1/2"	16'-2 1/2"	2 1/2"	
C502	4 of each	16'-11 1/2"	16'-3 1/2"	16'-2 1/2"	2 1/2"	
C503	4	26'-4"	9'-2"	3'-6"	2 1/2"	
C504	2 of each	16'-11 1/2"	16'-3 1/2"	16'-2 1/2"	2 1/2"	
C505	2 of each	16'-11 1/2"	16'-3 1/2"	16'-2 1/2"	2 1/2"	
C506	2	37'-9"	37'-9"	5'-3"	2 1/2"	
C101	50	23'-4"	23'-4"	5'-3"	5/8"	
C102	50	25'-1"	25'-1"	5'-3"	5/8"	
C401	50	23'-0"	23'-0"	5'-3"	5/8"	
C601	20	40'-0"	40'-0"	5'-3"	5/8"	
F701	49	24'-6"	24'-6"	5'-3"	5/8"	
F801	19	49	49	5'-3"	5/8"	
F901	35	25	25	5'-3"	5/8"	
F902	49	49	49	5'-3"	5/8"	
F903	25	25	11'-2"	8'-4"	3'-6"	2 1/2"
F904	25	25	17'-2"	14'-4"	3'-6"	2 1/2"
B615	2	21'-1"	6'-3"	3'-8"	3/8"	
B616	2	21'-9"	6'-7"	3'-8"	3/8"	
B617	2	22'-5"	6'-11"	3'-8"	3/8"	
B618	2	23'-3"	7'-3"	3'-8"	3/8"	
B619	2	24'-5"	8'-1"	3'-8"	3/8"	
B620	2	26'-0"	9'-1"	3'-8"	3/8"	
B621	2	29'-7"	10'-6"	3'-8"	3/8"	
B622	2	35'-11"	13'-8"	3'-8"	3/8"	
B623	2	35'-11"	13'-8"	3'-8"	3/8"	
B624	5	30'-9"	13'-8"	3'-8"	3/8"	

TABLE OF VARIABLES

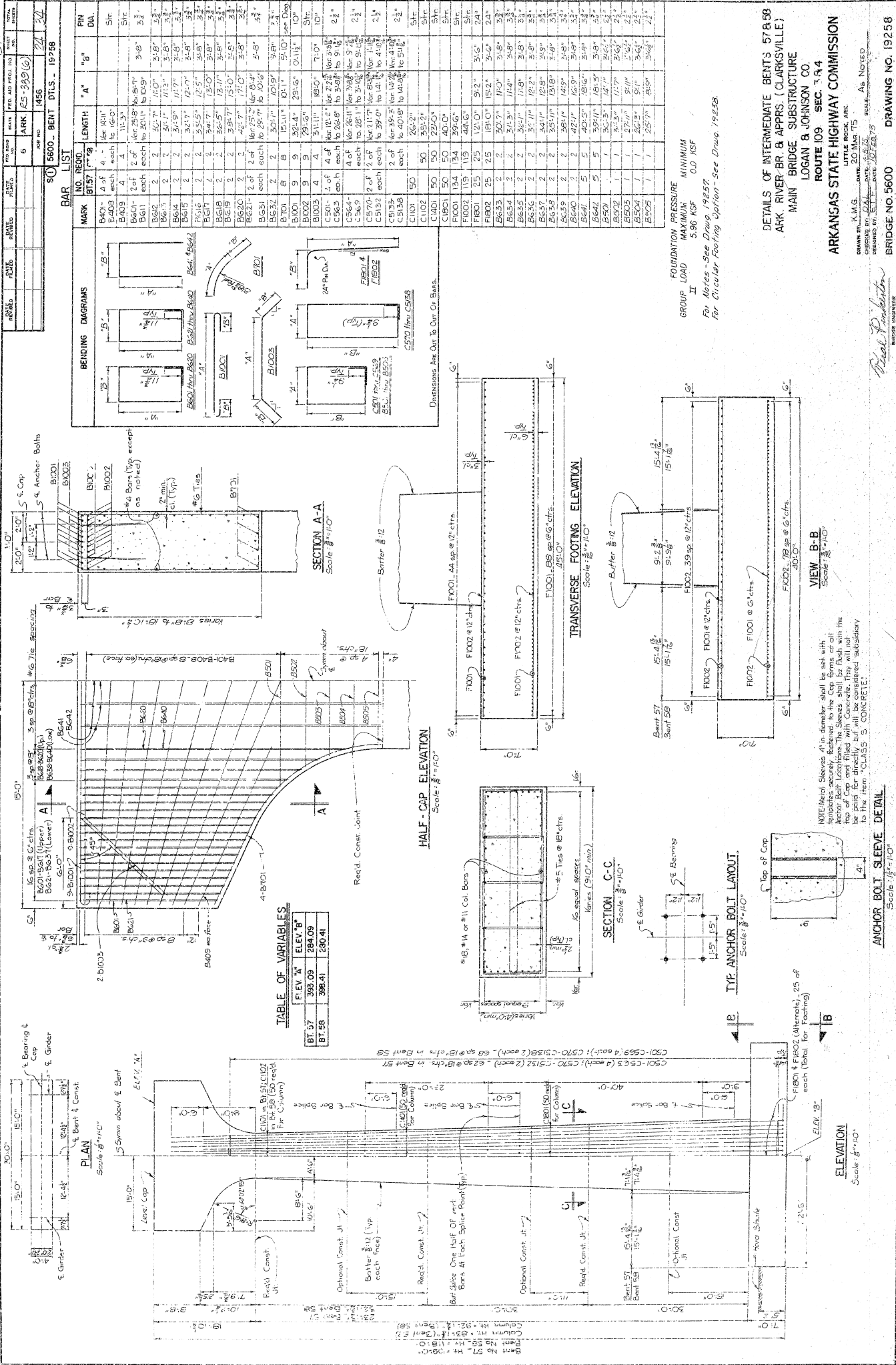
	ELEV "A"	ELEV "B"
Bent 56	393.58	290.05
Bent 59	405.20	302.70



NOTES:
AS AN ALTERNATE TO THE BUTT SPLICE SHOWN FOR #4 TO #11 BAR, A MINIMUM 9 FOOT LAP SPLICE MAY BE SUBSTITUTED AT THE CONTRACTOR'S OPTION.
DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1973 WITH 1974 INTERIM SPECIFICATIONS.

LIVE LOAD: HS20
METHOD OF DESIGN: LOAD FACTOR
CONCRETE: ALL CONCRETE SHALL BE CLASS 5 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH $f'_c = 3500$ PSI
CONSTRUCTION JOINTS SHALL BE HORIZONTAL OR AS SHOWN AND SHALL BE PROVIDED WITH KEYS NOT LESS THAN THREE INCHES HIGH COVERING THE MIDDLE ONE (1/3) OF BOTH DIMENSIONS.
REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615, OR A617, GRADE 60 YIELD STRENGTH $f_y = 60,000$ PSI
FOR BUTT SPLICING OF REINFORCING STEEL, SEE SP JOR NO. 1156, "BUTT SPLICING REINFORCING STEEL". BUTT SPLICING USING THERMAL WELDING SHALL BE USED WITH BARS CONFORMING TO ASTM A615 ONLY.
FOUNDATION PRESSURE
GROUP LOAD
MAX. 8.25 KSF
MIN. 41
For Circular Footing Option - See Drawg. 19257A

DETAILS OF INTERMEDIATE BENTS 56 & 59
ARK. RIVER BR. & APPRS. (CLARKSVILLE)
MAIN BRIDGE SUBSTRUCTURE
LOGAN & JOHNSON CO.
ROUTE 109 SEC. 3 & 4
ARKANSAS STATE HIGHWAY COMMISSION
LITTLE ROCK, ARK.
DESIGNED BY: K.M.W. DATE: 25 MAR 75
CHECKED BY: J.A.L. DATE: 1 APR 75
DESIGNED BY: E.T.F. DATE: 7 APR 75
BRIDGE NO. 5600 DRAWING NO. 19257

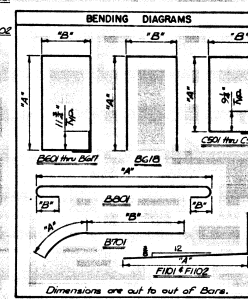
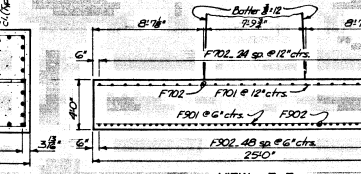
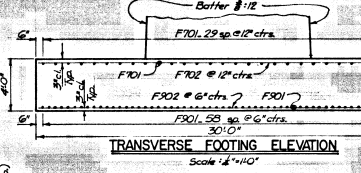
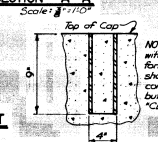
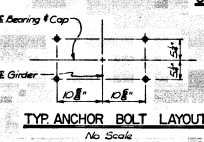
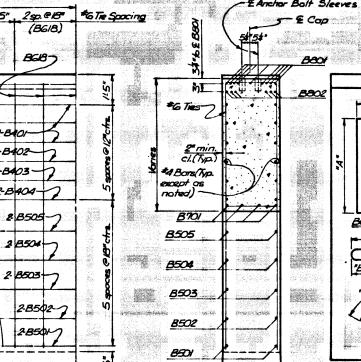
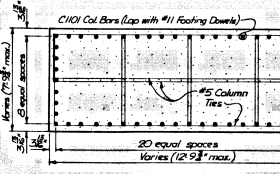
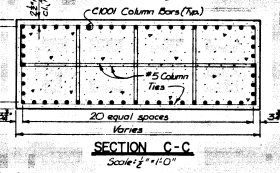
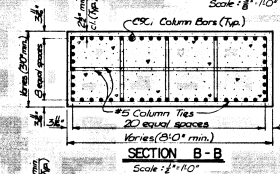
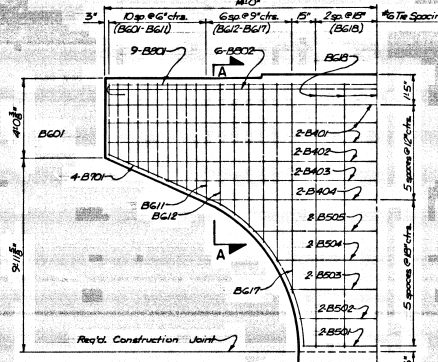
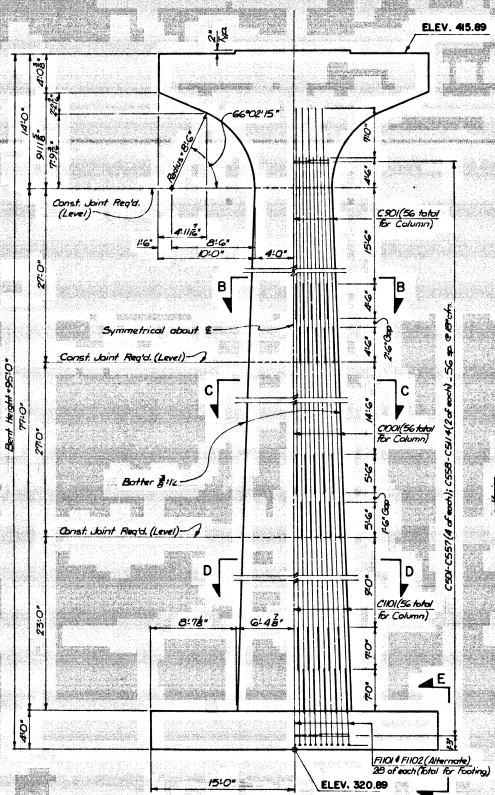
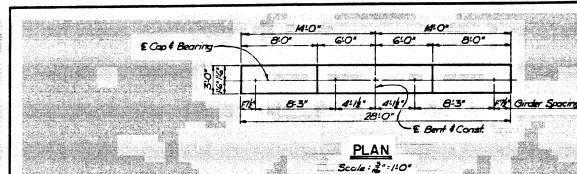


DETAILS OF INTERMEDIATE BENTS 57 & 58
 ARK. RIVER BR. & APPRS. (CLARKSVILLE)
 MAIN BRIDGE SUBSTRUCTURE
 LOGAN & JOHNSON CO.
 ROUTE 109 SEC. 3 & 4
 ARKANSAS STATE HIGHWAY COMMISSION

DESIGNED BY: K.M.G. DATE: 10/15/55
 CHECKED BY: J.H.E. DATE: 10/15/55
 APPROVED BY: J.H.E. DATE: 10/15/55

BRIDGE NO. 5600 DRAWING NO. 19258

As NOTED



BAR LIST						
MARK	NO.	REQ'D	LENGTH	"A"	"B"	PN DIA.
B401	2	27'0"				5/8"
B402	2	26'0"				5/8"
B403	2	21'6"				5/8"
B404	2	17'6"				5/8"
B501	2	17'0"				5/8"
B502	2	17'11"				5/8"
B503	2	8'9"				5/8"
B504	2	10'4"				5/8"
B505	2	12'10"				5/8"
B506	2	14'2"				5/8"
B507	2	15'8"				5/8"
B508	2	17'0"				5/8"
B509	2	18'2"				5/8"
B510	2	19'4"				5/8"
B511	2	20'6"				5/8"
B512	2	21'8"				5/8"
B513	2	23'0"				5/8"
B514	2	23'9"	7'6"	2'0"		5/8"
B515	2	23'4"	0'4"	2'0"		5/8"
B516	2	25'0"	9'5"	2'0"		5/8"
B517	2	20'9"	11'8"	2'0"		5/8"
B518	2	21'0"	12'9"	2'0"		5/8"
B519	0	15'4"	10'1"	0"		0"
B520	5	5'0"	27'4"	0"		0"
B521	2	9'7"				5/8"
B522	4 of each	14'0"	14'11"	14'3"	14'3"	5/8"
B523	4 of each	14'0"	14'11"	14'3"	14'3"	5/8"
B524	4 of each	22'0"	22'10"	22'6"	22'6"	5/8"
C101	56	31'6"				5/8"
C101	56	31'6"				5/8"
C101	56	20'6"				5/8"
F101	20	24'4"				5/8"
F101	25	17'0"				5/8"
F101	59	24'6"				5/8"
F101	49	29'6"				5/8"
F101	20	121'2"	101'6"	2'0"		14"
F102	25	19'2"	17'6"	2'0"		14"

NOTES

DESIGN SPECIFICATIONS: AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, 1973 WITH 1974 INTERIM SPECIFICATIONS.

LIVE LOAD: HS 20

CONCRETE: ALL CONCRETE SHALL BE CLASS 5 WITH A MINIMUM 28 DAY COMPRESSIVE STRENGTH $f_c = 3000$ PSI.

REINFORCING STEEL: REINFORCING STEEL SHALL CONFORM TO ASTM A615, OR A617, GRADE 60 YIELD STRENGTH = 60,000 PSI.

CONSTRUCTION JOINTS: CONSTRUCTION JOINTS SHALL BE HORIZONTAL OR AS SHOWN AND SHALL BE PROVIDED WITH KEYS NOT LESS THAN 3 INCHES HIGH COVERING THE MIDDLE ONE-THIRD OF BOTH DIMENSIONS.

SPLICING OF COLUMN BARS: BUTT SPLICES MAY BE USED INSTEAD OF LAPPED SPLICES IF BUTT SPLICES ARE USED THEY SHALL CONFORM TO SP OR NO. 1456 BUTT SPLICING REINFORCING STEEL. BUTT SPLICES USING THERMIT WELDING SHALL BE USED WITH BARS CONFORMING TO ASTM A615 ONLY.

FOR ADDITIONAL NOTES, SEE GENERAL NOTES D.W.G. NO. 18941

FOUNDATION PRESSURE: MAXIMUM 6.47 KSF MINIMUM 0.75 KSF

GROUP LOAD II

DETAILS OF INTERMEDIATE BENT 60
ARK RIVER BR. 8 APPRS. (CLARKSVILLE)
MAIN BRIDGE SUBSTRUCTURE
LOGAN & JOHNSON
ROUTE 109 SEC. 3&4
ARKANSAS STATE HIGHWAY COMMISSION

DESIGNED BY: K.M.G. DATE: 7 MAY 78
CHECKED BY: D.A.L. DATE: 5-2-78
DESIGNED BY: E.F.E. DATE: 11 APR 78
BRIDGE NO. 5600 DRAWING NO. 19259

